

TEREX ROAD

SITE IMPROVEMENT PLAN

SITUATED IN THE CITY OF HUDSON, COUNTY OF SUMMIT AND STATE OF OHIO AND KNOWN AS BEING PART OF ORIGINAL HUDSON LOT NO. 23.

FOR: THE TAYLOR RESIDENCE

BUILDER: PRESTIGE BUILDER GROUP
20 PARK LEDGE LANE
PENINSULA, OHIO 44264
PHONE: 330-802-8772

SITE INFO: P.N. 30-04425
734 BARLOW ROAD
HUDSON, OHIO 44236

NOTES:

MINIMUM FALL AWAY FROM HOUSE IS 6 INCHES IN 10 FEET.

MAINTAIN POSITIVE YARD DRAINAGE (TO SWALES, YARD DRAINS, NATURAL WATERCOURSES OR STREET) ACROSS.

VERIFY MINIMUM SLOPE OF 1% FROM BOTTOM OF FOOTER TO STORM AND SANITARY LATERALS. IF 1% CANNOT BE ACHIEVED, CONTACT CITY ENGINEER.

UNDERGROUND INFORMATION SHOWN TAKEN FROM IMPROVEMENT PLANS AND TYPE, SIZE AND ELEVATIONS TO BE VERIFIED IN FIELD BY CONTRACTOR PRIOR TO CONSTRUCTION.

CONTRACTOR MUST CHECK BENCH MARK WITH PERMANENT GRADE BEFORE ANY EXCAVATION OR WORK IS STARTED.

THE SITE SHALL COMPLY WITH "SPECIFICATIONS FOR INDIVIDUAL LOT PROTECTION" AND "EROSION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES".

THERE SHALL BE NO DISTURBANCE OF ANY KIND OR STORAGE OF MATERIALS TO OCCUR BEYOND THE SILT FENCE.

A/C UNIT WILL BE LOCATED OUT OF PUBLIC VIEW.

STEP FOOTERS AS NEEDED.

MUD, SOIL AND OTHER DEBRIS SHALL NOT BE DEPOSITED ONTO PUBLIC ROADWAYS. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF SUCH AS DIRECTED BY THE CITY OF HUDSON.

CONTRACTOR SHALL MINIMIZE IMPACT TO SURROUNDING RESIDENCES. VEHICLES AND EQUIPMENT SHALL NOT BE PARKED IN ROADWAY BLOCKING ADJACENT PROPERTIES.

CONTRACTOR TO VERIFY STORM AND SANITARY CONNECTION DEPTHS BEFORE BASEMENT EXCAVATION.

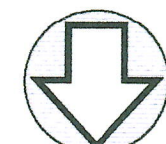
THIS PLAN FOR SITE PURPOSES ONLY, SEE FOUNDATION PLAN FOR HOUSE DETAILS. HOUSE PLANS USED DATED 10/17/24.

TREES (OR GROUPING OF TREES) SHALL BE PROTECTED BY 4' ORANGE CONSTRUCTION FENCE ERECTED AT DRIP LINE.

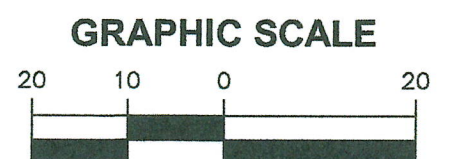
- XXXX* PROPOSED GRADE
- XXXXX EXISTING ELEVATION
- XXXX EX. CONTOUR
- XXXX PROP. CONTOUR
- INDICATES DIRECTION OF SURFACE WATER AFTER FINAL GRADING.
- BENCH MARK: TOP OF HYDRANT FRONT OF P.N. 30-04425 ELEVATION = 1022.41 (NAVD 1988)
- PROPERTY PIN SET OR FOUND
- SILT FENCE or SILT SOCK

IMPERVIOUS SURFACE COVERAGE:

POST-DEVELOPMENT 6,060 S.F. 16.0%



NORTH



(IN FEET)
1 INCH = 20 FT.

FEBRUARY 10, 2023

MAY 3, 2023

MAY 26, 2023

JULY 11, 2023

NOV. 12, 2024

NEW HOUSE PLANS

CONNECTIONS:

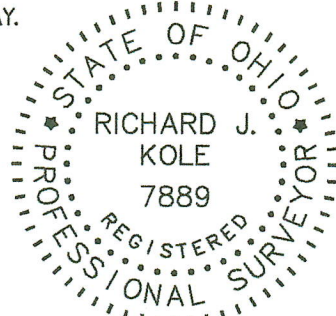
- (A) = USE 1" TYPE "K" COPPER
- (B) = USE 6" PVC SDR 35 w/ GASKETS 1% MIN., 10% MAX.

PREMIUM FILL FOR ALL UTILITY TRENCHES UNDER PROPOSED DRIVEWAY AND WALKS.

Dimensions shown hereon are expressed in feet and decimal parts thereof. Monuments were found or set as indicated hereon. Bearings shown are to an assumed meridian and are for the sole purpose of describing the relative angular value between lines, all of which I certify to be correct to the best of my knowledge and belief.

Richard J. Kole, Reg. Surveyor #7889 1/11/24 Date

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SERVICE
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(800) 362-2764



Prepared by:
RMKOLE
& ASSOC. CORP.
surveys • consultations • planners • utilities • cap
5316 Ridge Road - Cleveland, Ohio 44129
Phone 440.885.7137 - Fax 440.885.7139
www.kolesurvey.com
File No. 22305 Site

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CONTRACTOR TO VERIFY STORM AND SANITARY CONNECTION DEPTHS BEFORE BASEMENT EXCAVATION.

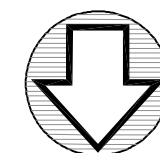
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- XXXX* PROPOSED GRADE
XXXX EXISTING ELEVATION
XXXX EX. CONTOUR
XXXX PROP. CONTOUR
INDICATES DIRECTION OF SURFACE WATER AFTER FINAL GRADING.
BENCH MARK: TOP OF HYDRANT
FRONT OF P.N. 30-04425
ELEVATION = 1022.41 (NAVD 1988)
PROPERTY PIN SET OR FOUND
SILT FENCE or SILT SOCK

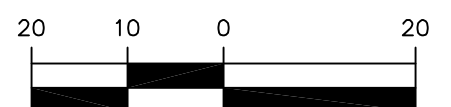
IMPERVIOUS SURFACE COVERAGE:

POST-DEVELOPMENT 6,060 S.F. 16.0%



NORTH

GRAPHIC SCALE



(IN FEET)

1 INCH = 20 FT.

FEBRUARY 10, 2023

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NOV. 12, 2024

NEW HOUSE PLANS

CONNECTIONS:

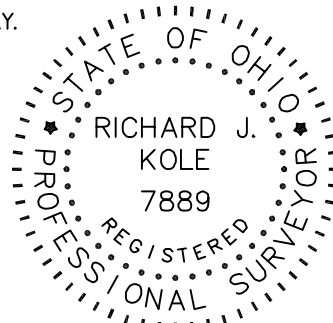
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1% MIN., 10% MAX.

PREMIUM FILL FOR ALL UTILITY TRENCHES
UNDER PROPOSED DRIVEWAY AND WALKS.

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Richard J. Kole, Reg. Surveyor #7889
Date 11/12/24



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INSTALL CONSTRUCTION ENTRANCE.
NOTE: CONSTRUCTION ENTRANCE
SHALL BE MAINTAINED AT ALL
TIMES AND SOILS SHALL NOT BE
DEPOSITED ON ADJACENT ROADWAY.

THE LOCATION OF EXIST. WATER CONNECTION,
IF ANY, IS UNKNOWN. NO RECORD INFORMATION
OR EVIDENCE OF LOCATION FOUND IN FIELD.
COORDINATE WITH HUDSON WATER DEPT.
(330-342-1710) IF NEW CONNECTION IS NEEDED.

BARLOW ROAD 60' (T.H. 121)

P.N. 30-00176
ANNETTE M. TYMCIO
746 BARLOW RD.

P.N. 30-04426
DORIS TAYLOR
724 BARLOW RD.

P.N. 30-04425

0.9660 ACRES
734 BARLOW RD.



FRONT ELEVATION
DRAFTSMAN RENDERING, GRADE MAY VARY

NOTE:
TRIM TO BE ALUMINUM AND VINYL,
EXCEPT AROUND GARAGE DOORS
TO BE AZEK

SIDING TO BE VINYL
SOFFITS TO BE ALUMINUM
PORCH CEILING TO BE ALUMINUM
PORCH POSTS/BEAMS TO BE AZEK



REAR ELEVATION
DRAFTSMAN RENDERING, GRADE MAY VARY

1st floor | 2,629 sq. ft.
total | 2,629 sq. ft.
Overall Dimensions: 68'-0" x 60'-0"

CUSTOM DESIGN FOR:
SPEC HOME
THREE CAR GARAGE, HUDSON OPTION
CONCEPT PLAN

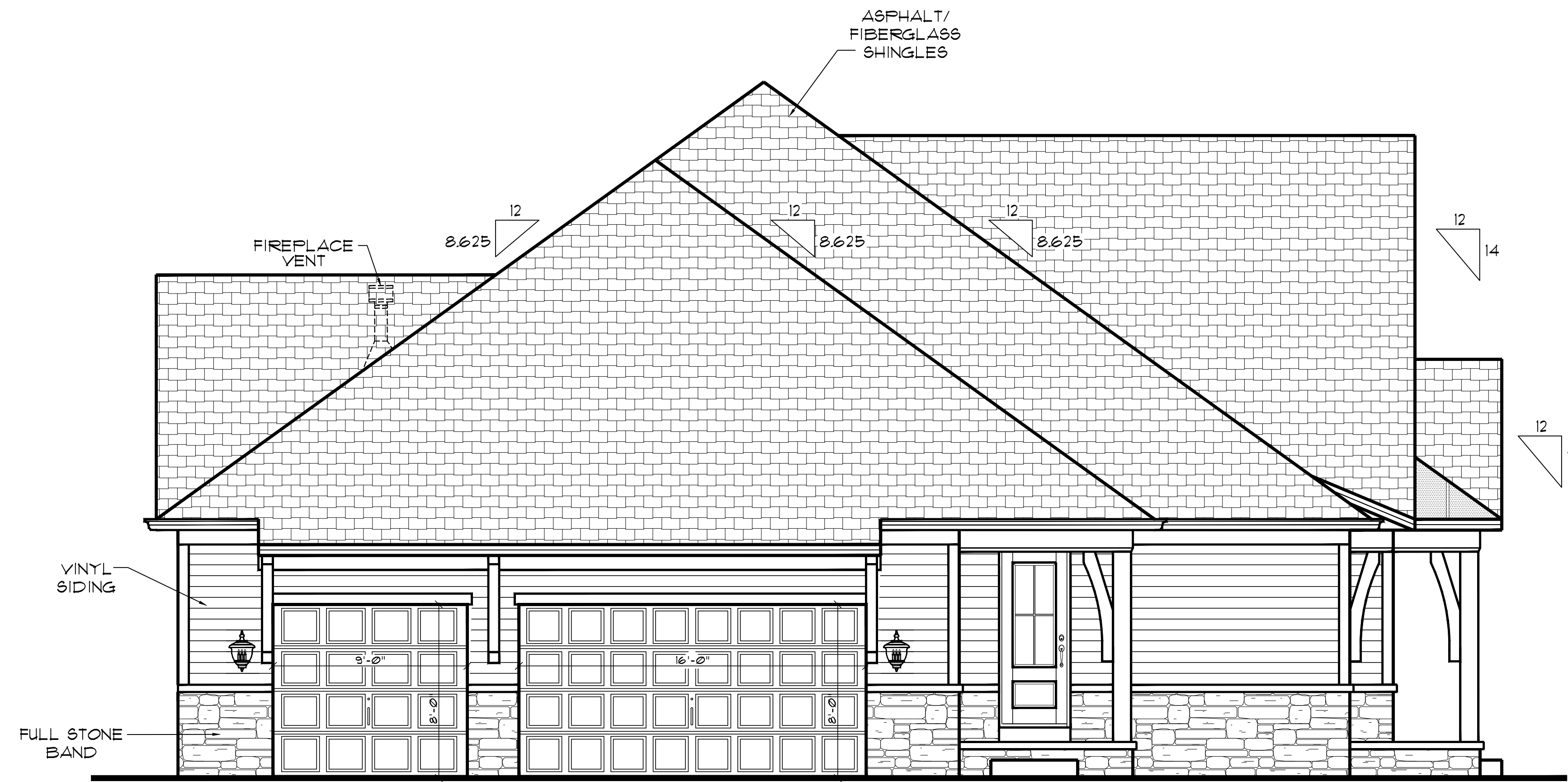
PRESTIGE BUILDER GROUP
Custom Home Design & Build
prestigebuildergroup.com

HOLLY RIDGE
FRONT AND REAR ELEVATION

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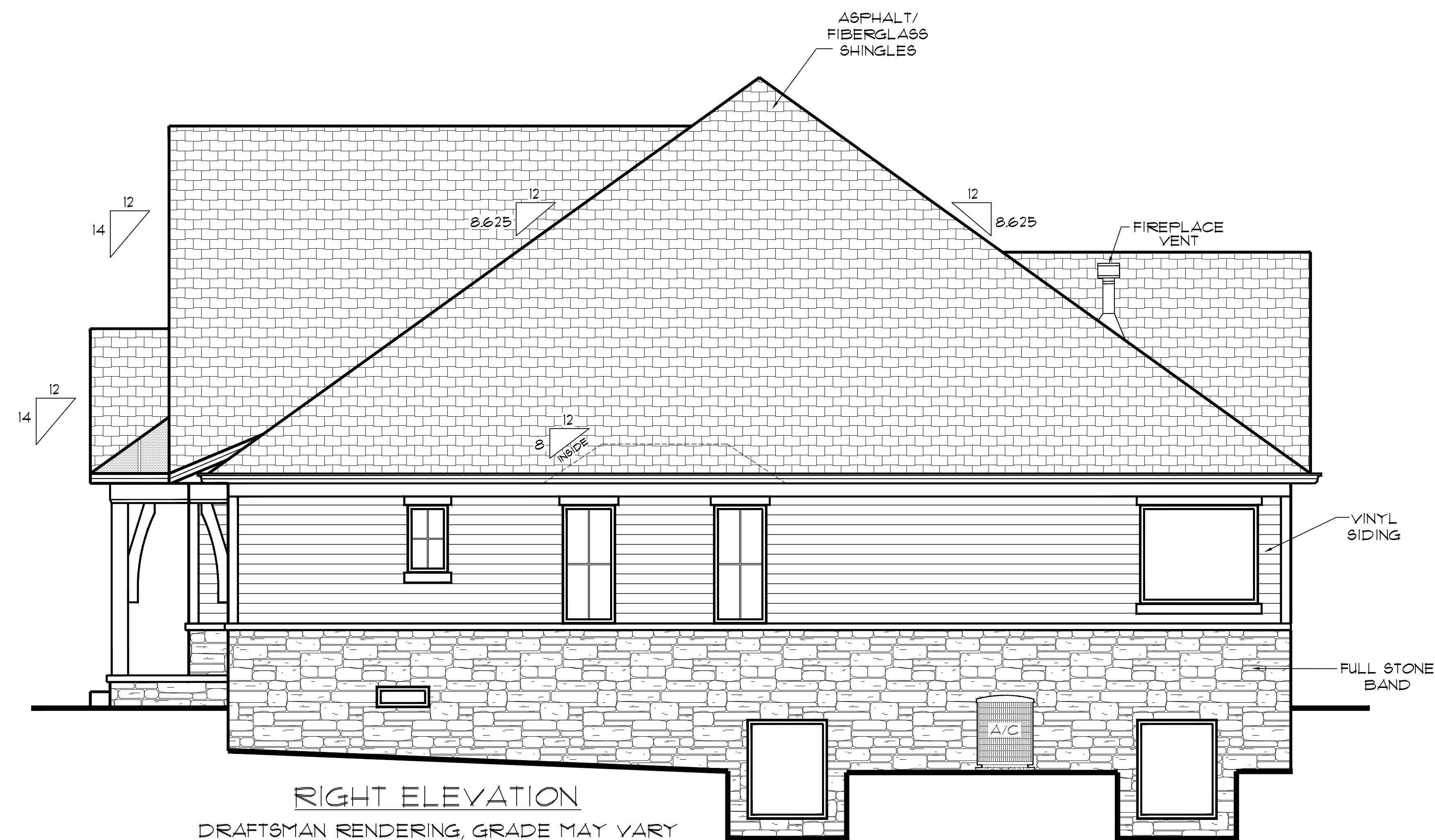
DRAWN BY: MAP
DATE: 10/17/2024

LIVING AREA:
2,629 SQ. FT.
SCALE:
1/4" = 12"



LEFT ELEVATION

DRAFTSMAN RENDERING, GRADE MAY VARY



RIGHT ELEVATION

DRAFTSMAN RENDERING, GRADE MAY VARY

CUSTOM DESIGN FOR:
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THREE CAR GARAGE, HUDSON OPTION
CONCEPT PLAN

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Custom Home Design & Build
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HOLLY RIDGE
SIDE ELEVATIONS

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2,629 SQ. FT.

SCALE:
1/4" = 12"



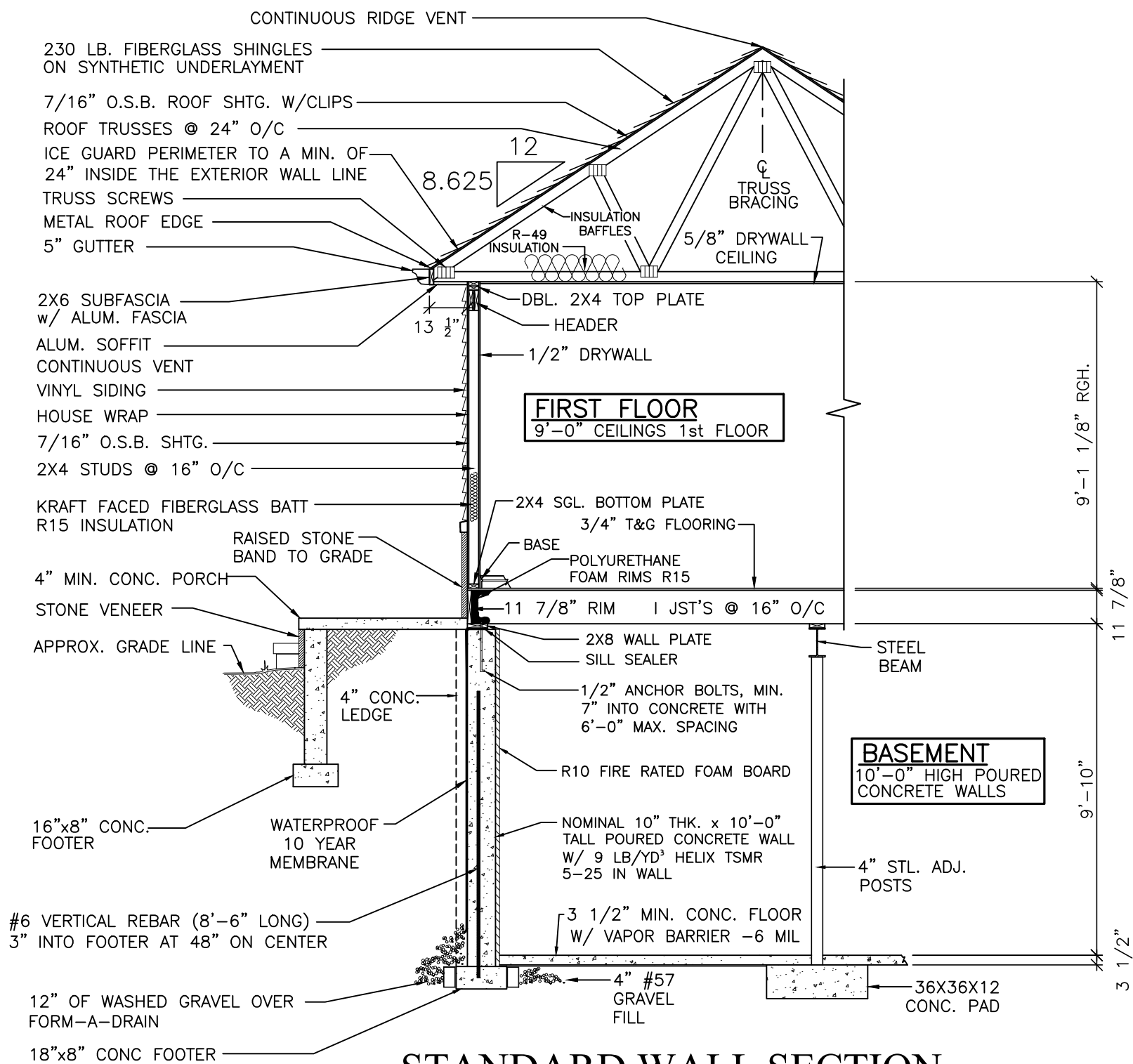
HOLLY RIDGE

FOUNDATION

FIRST FLOOR

SCALE: 1/8" = 1'-0"

HOLLY RIDGE



STANDARD WALL SECTION

SCALE: 1/4" = 1'-0"

ELECTRICAL NOTES:

400 AMP SERVICE
2-200 AMP MAIN BREAKER PANELS
PARALLEL &
UNDERGROUND

BRANCH CIRCUITS SHALL BE GFCI PROTECTED
IN ACCORDANCE WITH NEC 210.8

BRANCH CIRCUITS SHALL BE AFCI PROTECTED
IN ACCORDANCE WITH NEC 210.12

ARC FAULTS SHALL BE PER NEC 210.16.

OUTLETS TO BE TAMPER-RESISTANT PER
NEC 406.12

GROUND TO UFER AND COPPER WATER LINE
ENTERING THE HOUSE

NOTE:

2019 RESIDENTIAL CODE OF OHIO
ENERGY METHOD USED - OHBA PATH #1
(CHAPTER 11 SECTION 1112)
(INCLUDING ALL MANDATORY
REQUIREMENTS)

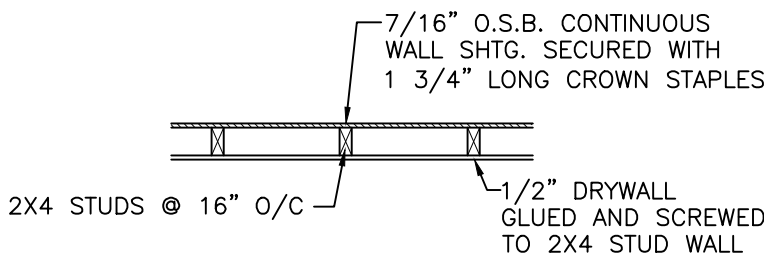
NOTE:

HANDRAILS AND GUARDS SHALL MEET THE
REQUIREMENTS OF RCO 311.7.7 AND RCO 312

WALL BRACING FOR ALL EXTERIOR WALLS TO BE
7/16" O.S.B. CONTINUOUS WALL SHEATHING
SECURED WITH 1 3/4" LONG CROWN STAPLES

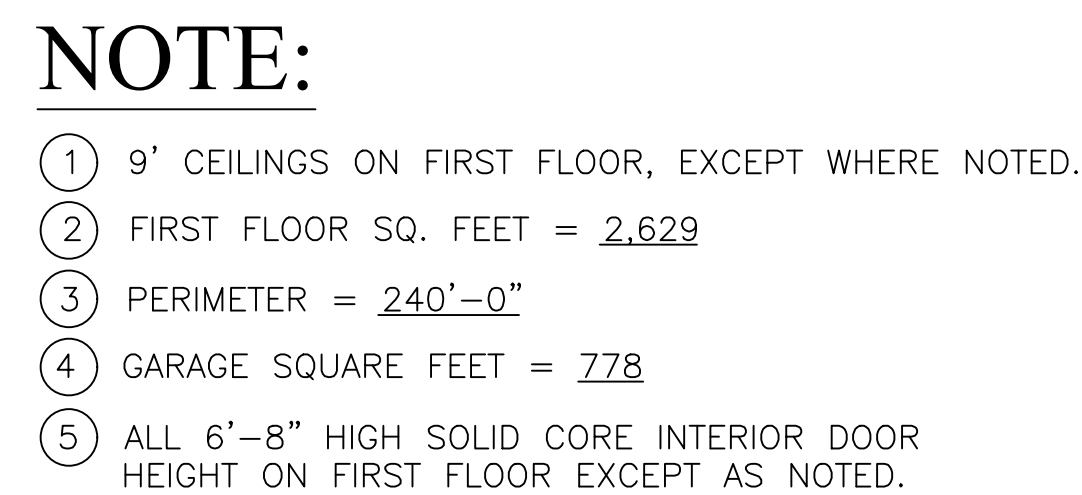
COLD AIR RETURN IN EVERY ROOM AS REQUIRED

SCHEDULE 40 PVC PIPING FOR DRAINS AND PEX
LINES FOR WATER LINES THROUGHT THE HOUSE



WALL BRACING DETAIL FOR ALL EXTERIOR WALLS

SCALE: 1/2" = 1'-0"



400 AMP SERVICE
2-200 AMP MAIN BREAKER PANELS
PARALLEL Ø
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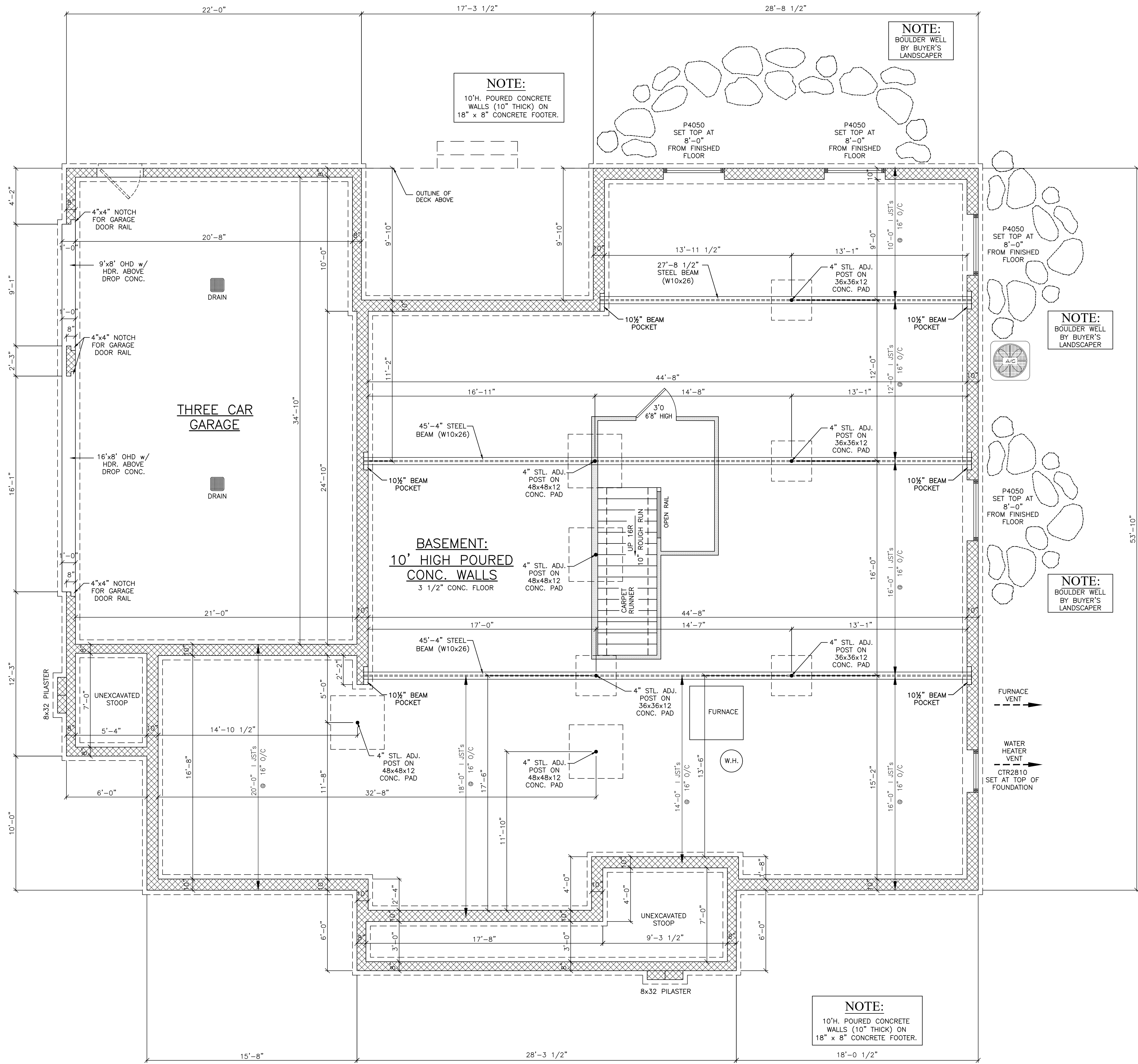
COLD AIR RETURN IN EVERY ROOM AS REQUIRED

SCHEDULE 40 PVC PIPING FOR DRAINS AND PEX LINES FOR WATER LINES THROUGHOUT THE HOUSE

U-FACTOR = .32	
CX16	2'-8" x 6'-0 3/8"
CX145	3'-0 1/2" x 4'-5 3/8"
C135	2'-0 5/8" x 3'-0 1/2"
P6050	6'-0 3/8" x 5'-0 3/8"
P3050	3'-0 1/2" x 5'-0 3/8"
A281	2'-8" x 2'-0 5/8"
AR31/CXW16	3'-0 1/2" x 2'-5 1/2"
CXW16	3'-0 1/2" x 6'-0 3/8"

4'-0" FRONT DOOR WITH 2'-0" SIDELITE (8' HIGH)	6'-2 3/4" x 8'-3 1/2"
3'0 SERV. DOOR (8' HIGH)	3'-2 1/2" x 8'-3 1/2"
CUSTOM TRANSOM/FWG12080-4 GLIDING PATIO DOOR (8' HIGH) (4) 3'-0" PANELS	11'-9 3/4" x 9'-5 3/4"

LIVING AREA: 2,629 SQ. FT.	SCALE: 1/4" = 1'
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NOTES:

- ① FULL STONE BAND
- ② FOUNDATION PERIMETER: 239'-8"
- ③ [Pattern] INDICATES FINISHED WALLS

ELECTRICAL NOTES:

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2-200 AMP MAIN BREAKER PANELS
PARALLEL &
UNDERGROUND

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GROUND TO UFER AND COPPER WATER
LINE ENTERING THE HOUSE

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COLD AIR RETURN IN EVERY ROOM AS REQUIRED

SCHEDULE 40 PVC PIPING FOR DRAINS AND PEX
LINES FOR WATER LINES THROUGHOUT THE HOUSE

WINDOW SCHEDULE: (ANDERSEN)	
U-FACTOR = .32	
CTR2810	2'-8" X 1'-0 1/2"
P4050	4'-0 1/2" X 5'-0 3/8"

CUSTOM DESIGN FOR:
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THREE CAR GARAGE, HUDSON OPTION
CONCEPT PLAN

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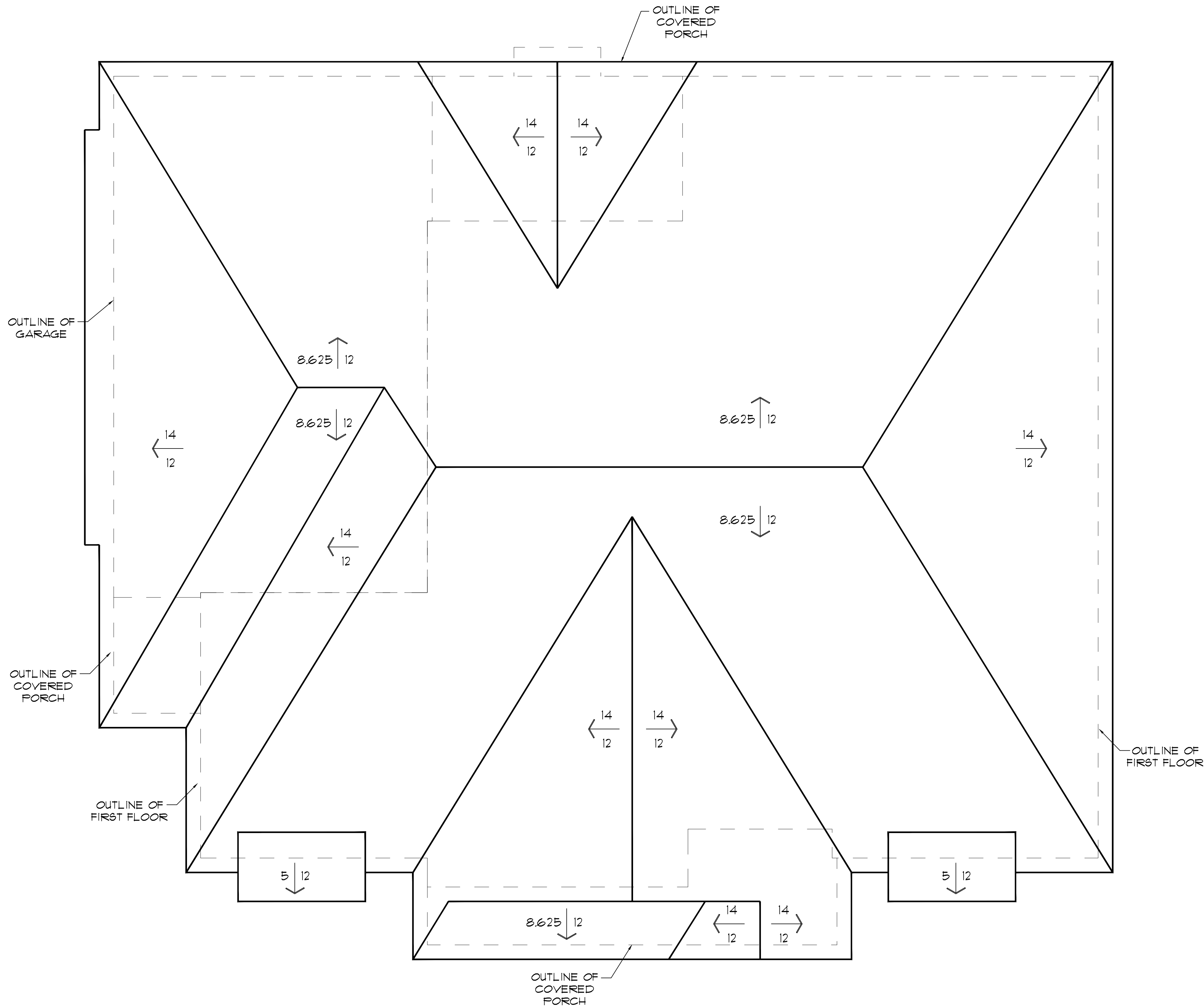
HOLLY RIDGE
FOUNDATION PLAN

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DRAWN BY: MAP
DATE: 10/17/2024

LIVING AREA:
2,629 SQ. FT.

SCALE:
1/4" = 12"



HOLLY RIDGE
ROOF PLAN

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CUSTOM DESIGN FOR:
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THREE CAR GARAGE, HUDSON OPTION
CONCEPT PLAN



670 Barlow Road – 3rd house west of project site.



712 Barlow Road – 2nd house west of project site.



724 Barlow Road – 1st house west of project site.



734 Barlow Road - Project Site



746 Barlow Road – 1st house east of project site.



760 Barlow Road – 2nd house east of project site.



780 Barlow Road – 3rd house east of project site.



745 Barlow Road – 1st house to the east of project site across street.



729 Barlow Road – House directly across street from project site.



715 Barlow Road – 1st house west of house across street.